



Important Information for YCC Yngling skippers and crew members

Dear Yngling Skippers and Sailors,

While this document is long, please make note of the following points.

If you are a member, you have the right to sail on our boats, but please take responsibility for helping to take care of them. Our club is run by volunteers to help everything run smoothly and to work well. While we try our best to solve any problems, big or small, we need everyone's help! This includes you!

If you see any potential problems or deficiencies on any of the Ynglings, no matter how small, or you have any comments or ideas, please let Denise (deniselew35@gmail.com) know, place a yellow warning on the boat log, and try to take responsibility for fixing the problem if you can. I am always happy to help you or answer any questions!

Please note that, if you need to get materials or supplies, you can go to **Moby Dick** next to the buvette or to the navigation shop, **SUI 4616, at the round-about of Mies/Tannay on the Route Suisse** (just 3 km north of Port Choiseul), where we have accounts so that you do not need to pay for anything yourself. If you do purchase anything else outside of these shops, keep the original receipt, let Denise know and you will be reimbursed.

Thank you to the many volunteers who have already helped this year!

FYI: Sauvetage du Lac (emergency): 112

WHEN GETTING THE KEY FROM THE COMPUTER

Please take note if recent boat users have left a yellow warning sign on the boat log:



Check recent entries on the log to see what the problem has been – if the problem is still there when you take out the boat, please make note of this

on the log when you return to port, and continue the Yellow Warning sign to skippers using the boat after you. It also lets the maintenance team know, so that we can address the problem. If you see a new problem, try to fix it yourself (there is a sail repair kit on every boat), or ask Denise for help.

GETTING THE BOAT READY

If sailing with an inexperienced crew, make sure that your crew knows how to tie knots correctly BEFORE you head out! Be sure to check all of their work throughout the outing, including coming back into port. Before leaving the mooring place, tie up the mainsail to the boom, and ask your bow crew, once they have released the jetty's black mooring lines, to make a simple loop around the jib to prevent it from falling off the deck and also making it easier to walk around when going to the bow to attach to the rigging buoy.

MOVING FROM THE MOORING SPACE TO THE RIGGING BUOY IN THE CHANNEL

Please note: It is not allowed to hoist sails in the parking places. And, unless you are very experienced, do not hoist either of the sails until you have reached one of the blue Greement Buoys in the main channel.

Teach any untrained crew to hold the paddle correctly, with the inboard hand on the top bar of the paddle (if the bar is broken, of course, this isn't possible), sit facing forward at the front of the cabin, but behind the shrouds, with the outer leg up on the side of the deck. In stronger winds, have your stronger paddlers on the leeward side of the boat, to prevent sideways drifting.

Slow the boat as you approach the buoy on your leeward bow. If it is windy, the wind can push you toward the buoy, rather than away from it. Have your crew simply loop the tail of the painter through the ring on the buoy and *wait for the boat to stop moving against the buoy*. Once the boat is stable, they can loop the painter back under the bar on the bow of the boat, and tie a bowline. There should be at least a 360-degree loop around the buoy and boat.

Hoisting the Mainsail (3 Ynglings on main jetty; Janisico has different method, below)

Please take care not to damage the mainsail luff when hoisting...

During the past years, the Yngling mainsails have needed much repair, as the **luff** edge (front edge) of the mainsails easily becomes damaged, if the sail becomes stuck when entering the mast and the halyard is pulled with force when **hoisting**.

Where the mainsheet exits the side of the mast, please ask one crew to climb up beside the mast to hoist. The angle is much easier, and the halyard will rub less on the frame of the mast. On Janisico, after hoisting or dropping the mainsail, please make sure that the halyard then is put up around the hook above the exit point on the mast - this prevents less rubbing and wearing of the halyard on the frame on the mast!

Pull the mainsail halyard smoothly, **taking care to have one of your crew hold and guide the luff edge of the sail directly into the track of the mast.** Another crew can tail the line to take up the slack and help the hoist.

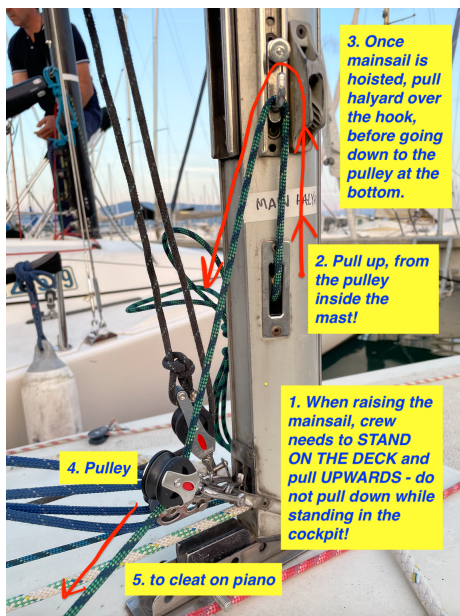
If the hoist becomes stuck at any time, stop and make sure that no part of the sail is caught while entering the mast. **If the luff tape shows any signs of getting stuck during the hoist or even slight damage, please place a warning on the log.**

For most sailing conditions, the mainsail should be slightly wrinkled (that is, *not hoisted to maximum tension*).

Hoisting the Mainsail on Janisico

Please note these special instructions:

1. Crew doing hoisting should stand *on the deck*, pulling the halyard UPWARDS from the exit point on the mast (note pulley helps this inside the mast), 2nd crew can take the slack from the tail. **DO NOT HOIST THE HALYARD BY PULLING DOWN FROM THE COCKPIT!**
2. When hoisted, put halyard over the silver hook before it goes to the pulley at bottom of the mast.
3. Cleat at the piano (note: the vertical cleat on the mast is worn out and needs replacing, June 2023)



Rigging and Hoisting the Jib

If any jib halyard is too tight and is difficult to hook at the base of the mast...

Usually, the **halyard** becomes twisted over time with repeated use, and becomes shorter and shorter as the twists become tighter. This means that it can easily get stuck while raising or lowering the **jib**. To solve this problem, lower the jib, undo the **shackle** at the **head** of the sail, and untwist the halyard *many, many* times until the halyard is flatter and evens out. It should then be easy to raise/lower and also to hook the second loop to the downhaul hook at the base of the mast. If you still have difficulty, please let us know.

Returning to Port after your outing

You must stop at the rigging/de-rigging buoys by moving **into the wind** (like stopping at the buoy when you are outside the port). Therefore, when you come back with wind from Lausanne, you may need to go around and below the buoy, then luff up and come back into the wind to stop. *Do not attempt to capture the buoy as you pass by going downwind* - it is too difficult to stop from this direction! Also, your crew should not hold the ring of the buoy with their hand, especially to stop the boat! They risk falling in the water or injuring themselves. **Go as slowly as you can, and just fast enough to stop quietly at the buoy!**

Do not yet put the wooden boom support in place, but wait until you are back in the mooring place at the jetty. It can otherwise fall into the water and also you cannot properly steer the boat.

BACK IN PORT AFTER SAILING

Skippers, please check that your crew has properly derigged and tidied the boat, before leaving to go home:

- Be sure to ease the following, to ensure the rig and sails are *not left under tension when the boat is not being used*:
 - **Backstay**
 - **Outhaul**
 - **Vang**

- Sails
- Halyards
- Mooring lines all carabiners attached to the boat
- Self-bailers, **always open** before you leave the boat.

- Fenders tied properly, *even if they have slipped before you first arrive* (see below). Do not tie fenders to the shrouds, unless you have to, and then, only at the very base. There should be no pulling on the shrouds!

Putting away the Mainsail



As our main sails on the 3 Ynglings are still quite new, please **roll** them before securing to the boom. On Janisico, you can either roll the main sail or flake it, as you would when you are preparing the boat to go out: use accordion folds, as in the photo, and when putting the boat away, use the last meter or so of the head of the sail wrapped around the whole sail to hold the folds.

You can either roll the mainsail starting at the **head** (try to keep the sail from twisting/crinkling) or if your crew can use **accordion folds** for the mainsail, as in the photo above, wrap the top/last meter or so of the sail entirely around the whole sail to secure it (not too tightly). Then, tie with elastic cables.


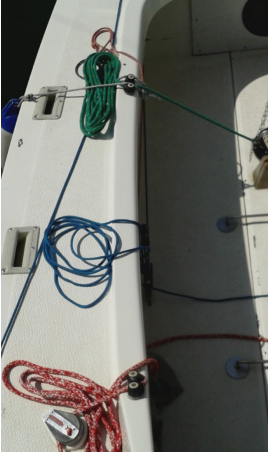

Before putting the cover on the boat...

Please **coil** and suspend the mainsheet from under the boom (or from the vertical mainsheet) so that it stays off the cabin floor. [Click here for a short video about how to easily coil and hang the mainsheet.](#) (Note: First, pull the mainsheet all to one side, then cleat and put a simple knot at the block to hold it, before coiling). This method makes it easier for the next crew to pre-feed the mainsail at the mast and keep the rest of the sail folded and tied until ready to hoist.

If you don't want to coil the mainsheet as above, please cleat the two mainsheet tails, then coil the rest of the sheets and lay them **on the deck** (sides or behind the mast), rather than leaving them lying on the floor or in the net, so that they stay drier and are easier for the next crew to access them. If they are in the net, they often become tangled in life-jackets, etc.

The boat cover should be tied to the mast and boom end, using **square knots**. Before leaving the boat, please check that there are six small fenders (three on each side), two paddles and three life jackets (four on Vas-Y and Janisico). If any are missing, take responsibility for noting this on the elog before you sign out.

Please also take responsibility for noting any other deficiencies on the elog, and offering to help fix them (you can ask Denise for help).

		
<p>Coiling and hanging the mainsheet from under the boom</p>	<p>Coiling sheets and laying on side of deck</p>	<p>Please leave cabin floor tidy</p>

'Painter' Jetty Cords

The '**painters**' (cordages that help crew get on/off the boats and that run from the bow to the jetty pole, lying in between the black tension mooring cables) are installed on all of the Ynglings. When back in port, please check that your crew uses this line correctly as described below.



One end of the **painter** should already be tied with bowline to the bow, close to the end of the line with a **bowline**. In port, the painters are looped around the pole, then back to the small bar on the bow, before being tied with a bowline (just as when tying up to the rigging buoy). NOTE: This **painter should be as slack as possible so that no tension is put on it.**



- Please be sure that you are in the correct parking space, as each boat has its own place that is colour-coded on the jetty pole, inside the boat and on the rear mooring buoy

Self-Bailers on Y Me and J'y Vais (see photo below)

Please leave the self-bailers open at most times!

The **self-bailers** sit above the water line on both sides of the **cockpit** and are there to help water drain automatically from the cabin of these boats, keeping the boat more or less dry. The Ynglings will not sink, even in good winds, as there is a lot of **polystyrene** in the hulls. Rob Veenhof says that, "In very light wind, it may be an advantage to keep them closed (less friction), **but otherwise, they should be left open at all times.**"

If the bailers are for some reason closed, use the heel of your foot to push the handle at the hinge outwards (you might have to push hard) until the bailer pops outwards – the bailer should then have an opening towards the **stern**. If you look from the outside of the hull, you can also see it open on the outside, just above the water level. **On these two 'smaller' Ynglings, please check that these are open before you leave the boat and go home!**



Self-bailer, open (see note above):

handle swings *away* from the opening and the bailer visibly protrudes outside the hull above the waterline. **On the two smaller Ynglings, please check that these are open before you leave the boat and go home!**

Additional, important information you need to know about sailing the Ynglings:

- Boats without engines (like the ynglings) are not allowed to go anywhere close to Geneva, because one can get caught in the strong current and won't be able to come back. By the way, there are very frequent controls by fast police motor boats.
- Boats without lights (like the ynglings) are not allowed to be on the lake after sunset. As above, there are very frequent controls by fast police motor boats and you will be responsible for any infractions.
- Ynglings are not equipped with an anchor as they are not required by law for this size of boat and there is insufficient space to store them safely on board. Do not ask for an anchor.
- If you want to stop the boat and swim, do a heave-to. **At least one person has to stay on board at all times. Please be aware that there is no ladder and getting back into the boat is not easy for many people!!!** Make sure the person on board is able to recover you, or, at least, hold a rope connected to the you. Take into account that an hove-to boat can still drift much faster than you can ever swim.
- Leave the boat in a clean and tidy condition, and take all your garbage with you.
- **If there are small rips or tears in the sails, please take a moment to repair them and note this in the e-log. If there are any other problems that you see, please be sure to make note of them on the e-log. We would greatly appreciate your help with their repair (ask Denise for help), as the Maintenance Team are all volunteers who spend much of their time helping to keep the boats in good condition for everyone to use.**

Leaving a comment on the boat log

When putting the key back into the computer closet, it is your responsibility to leave a note that includes the names of your crew, sailing conditions, if this is a private outing (with friends/family), and especially *any potential problems* that others sailing the boats should be aware of. Empty elogs or elogs with just "boat ok" are not complete. Click the [yellow Warning](#) so that other Yngling sailors will be aware.

Sail Repair Kit

If you see that the sails need repair, please take responsibility for helping to fix them. There should also be a spare **shackle** in this kit. If you use the spare shackle or if the roll of tape is coming to the end, please buy a replacement from Moby Dick or SUI 46.16 on the YCC account. The reference information is on the card in the box (see photo below). If items are missing, please let the Denise and/or the Maintenance Team know.

	
<p>There is a sail repair kit for each Yngling.</p>	<p>Spare shackle</p>

...

YCC SAIL REPAIR KIT

Please use the tapes and scissors in this kit to repair minor damage to sails.

- The transparent tape is for windows in the sails (BW0900-MY)
- The black Nylon tape is for spinnakers (BW090-N)
- The white Dacron tape is for main, jib and genoa sails (BW095)

PLEASE ALSO TRY TO IDENTIFY WHY THE SAIL MIGHT HAVE BEEN DAMAGED AND TAKE ANY ACTION TO AVOID FURTHER DAMAGE

Please log on the system:

- A brief description of any damage and the repairs you were able to make
- An indication whether you feel the sail should also be seen by the sail maker
- If in doubt send a photo to ycc-sail-maintenance@cern.ch

Finally, if any of rolls is coming to an end, please buy a new roll from Moby Dick or SUI4616 to restock the kit. The numbers in brackets above are the Bucher Walt catalogue references which both shops use and all you have to do is sign for them.

When you earn your Yngling key, we hope that you have fun sailing with our club. If you have not sailed much, previously to joining the club, please be honest with

yourself about your abilities and skills. You may need a lot more practice to really be confident in handling a boat - if so, please ask other key holders to go out with you to practise. If you want to take the Y test, please be aware that we expect a high degree of expertise, and do not give the key so that you can go and practise afterwards.

Also, be aware that winds on Lac Léman can quickly change and become very strong. Always check the weather and wind conditions before leaving the jetty. Try to check all three of the following apps , as relying on just one is not always enough. If you have any reservations about your and your crews' safety, please listen to your instincts and stay safe. You have the right to use our Ynglings, but with that right comes the responsibility of treating our boats with respect.

- **Windfinder:** search Versoix (Lac Léman)
 - Click Forecast/Superforecast
 - Click 'Report/Observation' to check actual, current wind conditions and to verify against the forecast.
- **MétéoSuisse:** search Versoix
 - Check Animations' for potential storms approaching (this can also be done while out on the lake, if conditions look threatening).
- **Windy.com** (or Windy app): Versoix
 - At the bottom of screen, beside "Basic", click "Wind" which shows FIVE models of forecast from around Versoix, giving more possibilities and info.

Different members swear that one app is better than another, but if you check all three of these resources, you will, as skipper, be more informed about possible conditions.

Below, you will find a basic Glossary of sailing terms. If you have any questions or comments about the boat and/or equipment, if you notice anything that needs repair or replacing, or if you have any suggestions to make our web page better, please let us know.

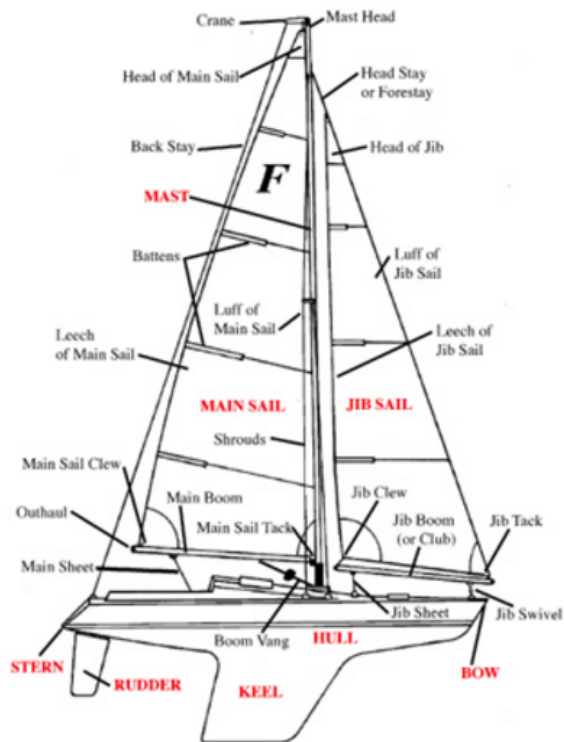
Please take a little extra time to help to take good care of our boats, as they will last longer and be more enjoyable for everyone to use!

Thanks for your help!

Silvia Costantini and Denise Lew
2023

Glossary

Here are some common sailing terms and their definitions. For a more complete list, look online or click here for one such source: [NauticEd](#)



Above Image: SailBakersfield.com


Accordion fold: When finished sailing, fold the mainsail along the boom from side to side, so that the sail drapes about 30 cm on each side of the boom. Crew at the leech end of the sail should be sure to always pull the leech and sail back towards the stern, to keep the sail flat and smoother, as it is folded.

Backstay: A support wire that runs from the stern to the head of the mast.

Bailer: A device used to remove water from the boat.

Berth: Parking place in the port

Bow/Stern: The bow is the front of the boat, the stern is the back. Anything near the front of the boat is referred to as being “forward,” and anything toward the back is “aft” or “astern.”

Grommet:  metal ring at the head, tack or clew of a sail.

Halyard: Lines that are attached to the head of a sail and used to hoist sails up the mast.

Head: Top of sail

Heeling: This is the term for when a sailboat leans over in the water, pushed by the wind.

Helm: From where you steer the boat. On smaller boats, like an Yngling, it is at the tiller, which is basically a long wooden stick. It is used to control the boat’s rudder.

Hoist: To raise the mainsail or jib by pulling on the main or jib halyard

Jib: The front sail on boats with two or more sails, and unlike the mainsail, does not have a boom.

Gybe (also Jibe): Changing tacks while sailing down wind, bringing the stern of the boat through the wind.

Keel: The keel is a long, heavy fin on the bottom of the boat that sticks down into the water. It provides stability and is the reason why modern sailboats are nearly impossible to capsize.

Leech: The back edge of a sail.

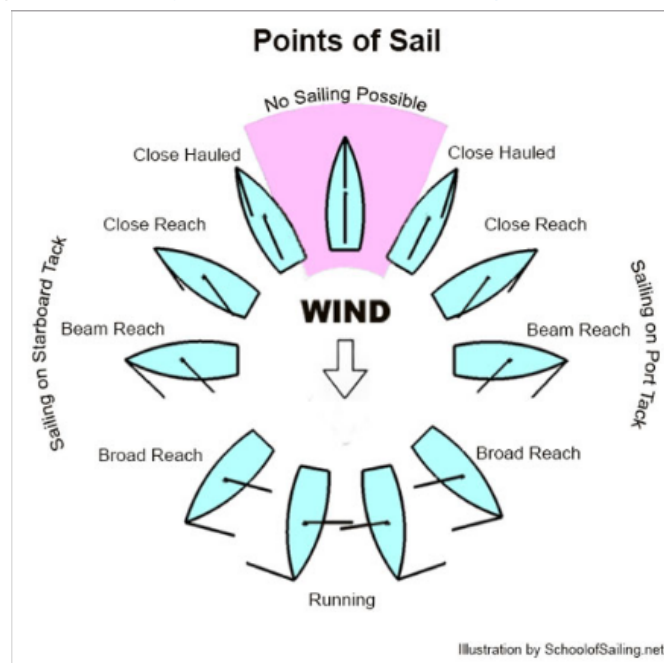
Leeward: The side of the boat furthest from the wind. When heeling over, this will always be the low side.

Luff: Forward edge of a sail.

Mainsail: The big triangular sail just aft of the sailboat's mast. As the name suggests, this is the boat's largest and most important sail. Running along its bottom edge, the mainsail has a thick pole called the boom.

Painter: mooring lines from bow to the dock and back to the bow, used for boarding the boat. This line should be left very slack, almost touching the water. See photo of painter above.

Point of Sail: The boat's direction relative to the wind. There are 8 commonly used points of sail. Here is one example showing the different [points of sail \(schoolofsailing.net\)](http://schoolofsailing.net):



Sheet: On a boat, this is what you say instead of "ropes."

Shrouds: Standing rigging that runs from the top of the mast to the sides of the hull.

Tack: This term has two distinct meanings, both of them very important. As a verb, to tack is to change direction by turning the bow of the boat through the wind. As a noun, your tack is the course you are on relative to the wind. For example, if the wind is blowing over the port side, you are on a port tack. If it's blowing over the starboard side, you're on a...you guessed it...starboard tack.

Windward: The side of the boat closest to the wind. When heeling over, this will always be the high side.

Beaufort Wind Scale

[Click here for one example of the Beaufort Wind Scale](#). There are many others that can be found online.

Principal editors:

Denise Lew (2017-2023)

Silvia Costantini (2017-2018)